



**NORTH HERTS LOCAL PLAN
EXAMINATION SUBMISSIONS**

**SITE LG1 – LAND TO THE NORTH
OF LETCHWORTH GARDEN CITY**

**PLANNING SUMMARY
STATEMENT**

JANUARY 2018

**Letchworth Garden City Heritage
Foundation**

Foundation House
Icknield Way
Letchworth Garden City
Herts
SG6 1GD.

T 01462 476007

www.letchworth.com

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1.0 INTRODUCTION

- 1.1 This Planning Summary Statement has been prepared by Letchworth Garden City Heritage Foundation (LGCHF) as land owner, in partnership with North Hertfordshire District Council (NHDC).
- 1.2 This Statement is in support of the proposed allocation of land marked as LG1 in the draft North Herts District Council Local Plan.
- 1.3 The proposed allocated land LG1, “the site”, has been identified by NHDC as a site for strategic housing growth to the north of Letchworth Garden City. Policy SP15 in the Local Plan allocates the land for 900 new homes and both NHDC and land owner, LGCHF, consider the sites to be available and deliverable in line with the Council’s housing trajectory.
- 1.4 This Statement has been prepared to assist the Inspector to understand how the development of this site may come forward and its deliverability. It supports the aspirations of Policy SP15 and Site Allocation LG1.
- 1.5 It provides information on earlier discussions undertaken by LGCHF with key stakeholders and initial thoughts on how the site may come forward for development, along with a programme of actions leading to the anticipated submission of a Planning Application in 2020 and first site dwelling completions in 2023.

2.0 THE SITE

- 2.1 The site lies to the north of Letchworth Garden City adjacent to the Grange Estate and the Letchworth Garden City Greenway.



Site Location – LG1

- 2.2 It is approximately 45 hectares (110 acres) in area and is presently utilised for agricultural purposes. A site location plan is provided at **Appendix A**.
- 2.3 The site is under the freehold ownership of LGCHF and there is a current agreement with a commercial farmer who is responsible for farming this land as part of a wider Farm Business Tenancy Agreement. This Agreement has been designed in order to support the development of this land should it become available. Otherwise, there are no encumbrances on the ownership of the site.
- 2.4 The site incorporates a series of hedgerows, which will be retained as part of the development of the site.
- 2.5 It is bordered on all boundaries by open farmland with the exception of to the south which is bounded by existing Local Authority playing field facilities and residential dwellings forming part of the Grange Estate development. It lies in close proximity to

the Letchworth Garden City Greenway, which is maintained by LGCHF as one of its charitable commitments to the local community.

2.6 The site has no relevant planning history.

3.0 PROPOSED DEVELOPMENT

- 3.1 It is the intention of the Heritage Foundation to comply with the policy provisions set out in Policy SP15 of the draft Local Plan, which sets out the requirements sought by NHDC as part of this development.
- 3.2 This policy identifies the site as a strategic housing development site for approximately 900 new homes. It places a requirement on a master planned approach, which shows how the site will follow and implement Garden City Design Principles. It requires an understanding of vehicular access requirements and consideration of landscape and traffic impacts.
- 3.3 The policy places a presumption in favour of a new primary school to be provided on site and seeks to ensure that the development will ensure that there is not a significant impact on nearby Heritage Assets.
- 3.4 It also includes a requirement for a neighbourhood retail and community facility, at least 9 plots for self-build development, sustainable urban drainage, structural planting and the diversion of provision of the Letchworth Garden City Greenway.
- 3.5 LGCHF agrees to all of the requirements incorporated within this policy.
- 3.6 Furthermore, there is also agreement to the provisions of Policy HS2 regarding the provision of affordable housing, LGCHF is committed to providing 40% affordable housing provision in line with the amended definition of Affordable Housing, as agreed when Matter 8 was discussed at the Public Examination to include reference to Community Land Trusts. This will include traditional socially rented and shared ownership models, as well as community land trust, local community and cooperative housing models as part of the intermediate provision.
- 3.7 The discussion at the Examination with respect to Matter 8 also raised the need for a more detailed understanding of local housing need requirements within Letchworth Garden City.
- 3.8 Provided at **Appendix B** is an initial illustrative plan, which includes the development site area, most likely parcels of housing development land, an identified indicative school site following discussions with Hertfordshire County Council Education

Authority, a sustainable transport hub and community centre and the proposed retained and relocation of the Letchworth Garden City Greenway.

- 3.9 This indicative initial plan shows the intention to retain the existing ancient hedgerows outside of the site, as well as the additional hedgerows within the development area.
- 3.10 This plan also highlights a clear opportunity for a phased approach with more than one outlet operating at any one time.

Transport and Connectivity

- 3.11 In addition, the indicative plan shows initial thoughts on the most likely vehicular access routes into and out of the site as well as the pedestrian and cycle routes.
- 3.12 In support of this indicative plan, at **Appendix C** we provide a Summary Report by Steer Davies Gleave, the LGCHF's appointed Highways Consultants, setting out their initial views with respect to the transport strategy and access to serve this site. This requires detailed modelling with Hertfordshire Highways, but provides access to the north, which will also be used for construction traffic to avoid disturbance to residential occupiers and a new access from Western Way in the Grange Estate. The latter includes land under the ownership of North Hertfordshire Homes. They have indicated an in principle support to finding an appropriate access solution (**Appendix I**).
- 3.13 The development will be seeking to provide opportunities for linkages from The Grange estate and the proposed new area into the town centre and employment areas, which presently form part of Letchworth Garden City. This will be by way of providing enhanced public transportation, cycle links and pedestrian routes. It also generates opportunities to provide pedestrian and cycle routes to nearby Fairfield area to the west of the development site. Steer Davies Gleave have commenced a study working with the local community and statutory authorities, to provide opportunities for town wide cycle network improvements, facilitated by this development, including improvements for the Grange estate residents and the whole of the town. It is also hoped that this will help support Hertfordshire County Council's aspirations for a model shift in transport usage.

Drainage Matters

- 3.14 As part of the development of the site, it is our intention to provide a sustainable urban drainage system and initial discussions have already taken place with Anglian Water, who is the waste water authority for this area (see Appendix D). This will form part of the master plan approach of the site creating interesting habitats, which will be of ecological value as well as providing a technical solution to surface water requirements, which working with Anglian Water, will provide an opportunity for an exemplar scheme.

Landscape Strategy

- 3.15 This drainage provision will also link in to a landscape strategy, as part of the master plan approach, which will incorporate significant buffers to the edge of the development site preventing further expansion as well as reflecting Letchworth Garden City Design Principles, which encompass tree line boulevards and significant structural and garden planting. The development will also incorporate wildflower meadow, as well as functional open space. It is also expected that urban agriculture will form part of the master plan, as well as allotments and community gardening.
- 3.16 As indicated in the indicative plan provided at **Appendix B**, the proposed relocation of the Greenway will retain the existing route, but also create a diversion around the edge of the development site, which will provide an attractive alternative for users.

Local Amenities and School Provision

- 3.17 The existing Grange playing fields lie outside of the site boundaries, but the proposal provides an opportunity for this to be enhanced and act as a focal point for both the existing Grange community as well as the occupiers of the proposed new dwellings. The development also generates opportunities for this to be improved, creating linkages between the existing and proposed residential areas.
- 3.18 Within the central core area identified on the illustrative plan, it is the intention to include community and retail facilities in close proximity to the proposed new school area, which will also be linked into a sustainable transport hub which will also provide non-car travel opportunities for existing Grange residents as well as the occupiers of the new dwellings.

Housing Matters

- 3.19 The overall proposed site density is 20 dw/ha across the whole of the strategic allocation, which provides an opportunity for the scheme design to include a range of housing densities and tenures, as well as open space opportunities, which will be of benefit to existing and new occupiers. Included as a background paper to LGCHF's Regulation 19 representations, is a Local Housing Report, undertaken by Lichfields, which provides an initial understanding of local housing requirements. This will be developed in partnership with NHDC to ensure the most appropriate housing is provided to meet the needs of the local community and economy. It is also expected that self-build plots will exceed the 9 included in the Approved Local Plan SP15 Policy and provided opportunities for local people.

Design Matters

- 3.20 It is envisaged that the layout of the development will reflect Garden City design principles, but it is LGCHF's intention to undertake a design competition "Reimagining The Garden City", in partnership with the RIBA, which will highlight how sustainable design interpreting original Garden City design aspirations can be achieved, set within a layout which reflects the early master planning attributes advocated by Parker and Unwin.

4.0 BACKGROUND TECHNICAL REPORTS

- 4.1 LGCHF, as part of a due diligence process and also to support the allocation, has undertaken a series of reports.
- 4.2 As previously mentioned, Steer Davies Gleave has undertaken an initial study of the site from a transport perspective a summary of which is provided at Appendix C.
- 4.3 Provided at **Appendix E** is a Summary Report from Ricardo Consulting with respect to the ecological implications of the proposed development. This highlights that subject to some mitigation measures, including the retention of existing hedgerow and trees, the development will not have a significant impact on local ecology systems.
- 4.4 There was discussion in earlier sessions at the Examination regarding Matter 19 about the presence of corn bunting on the land and ancient hedgerow. Our ecologists have not recorded any corn bunting specifically on any of the surveys at the site thus far and the biological records obtained in 2016 include one record of corn bunting, located 2km south-west of the site. This will however be examined in more detail during the winter and summer bird surveys, already instructed. The ancient hedgerow is located outside the site boundaries and its integrity will not be harmed by the development proposals.
- 4.5 Ricardo have also undertaken Flood Risk and Hydrology Assessment, updating a previous study in 2013, which also indicates that the development should not have a significant impact, in these aspects of the proposal. A copy of this summary is provided at **Appendix F**.
- 4.6 Archaeological Solutions have provided an initial desktop analysis regarding potential archaeological implications of the development, a summary of which is provided at **Appendix G**. This indicates that as expected in a location of this nature, ground investigations will need to be undertaken. This is fully supported by the LGCHF who has supplied this information to Hertfordshire County Council Archaeological Officer and will be undertaking further investigative work, around the needs of the current farm operation.
- 4.7 All of these matters will be considered in detail as part of a collaborative design process, working with NHDC, the local community and other key stakeholders.

Utility Providers

- 4.8 An initial report has been provided to understand the utility requirements to serve a development of this nature in this location. This is provided at **Appendix H** to this statement.
- 4.9 This demonstrates that at this early stage of the process, the site is deliverable in accordance with the provisions of the Draft Local Plan.
- 4.10 In particular, there is sufficient network capacity within UK power networks and the local gas infrastructure is envisaged to be capable of supplying the needs of the proposed development.
- 4.11 An application will be made to Affinity Water regarding the water supply, however, it should be noted that the overall sustainability approach for the development will be to seek to minimise the use of mains water, with water recycling to be part of the design concept.
- 4.12 As part of the Local Plan process, Affinity Water has been contacted by NHDC and as indicated in **Appendix D**, the waste water authority has already been contacted by LGCHF.
- 4.13 There is also existing infrastructure with respect to telephone and internet services, and as part of the development of the site, the LGCHF will be seeking high speed broadband to encourage a more sustainable and modern pattern of living.

5.0 DELIVERABILITY

5.1 LGCHF supports the NHDC's housing trajectory set out in Examination Document 3 (ED3), which indicates the first of the dwellings to be occupied in 2023 with a trajectory of approximately 100 dwellings per annum. We consider that this is achievable and would expect more than one outlet to be on site at any one time. It should be highlighted that in addition to private developers, the 100 dwellings per annum will include affordable housing provision. This will include local Housing Association providers, who have indicated their support of this allocation provided at **Appendix I**, as well as community groups who will form part of the intermediate provision by way of community land trusts and cooperative forms of housing. This will provide a range of housing tenures and opportunities for local people as well as providing development that will help to sustain the local economy.

Social Infrastructure Provision

5.2 The scheme viability affords a reasonable and related contribution to local infrastructure, linked to the Strategic Allocation LG1.

5.3 With respect of local infrastructure, the LGCHF has commenced discussions with the Hertfordshire County Council's Education Team and the County Council has set out its requirements for a new two-form entry primary school to be provided on site. Several locations have been provided as possible locations for this proposed new school, details of which are provided at **Appendix J** to this Statement. One of the possible locations is included in the indicative Master Plan provided at **Appendix B**.

5.4 LGCHF is supportive of meeting local requirements as part of this development.

5.5 Across the suite of infrastructure requirements included in the Infrastructure Delivery Plan 2016 and the Infrastructure Delivery by Location table appended to the Council's Matter 6 Statement, we support the proposed infrastructure improvements in this table, including the provision of additional GP facilities, by way of pooling contributions across the Letchworth Garden City sites. We look forward to working with the various local authorities, the local community and other key stakeholders to ensure that these are delivered, supporting the Grange Estate residents, as well as the occupiers of the new development. As LGCHF owns the freehold of the Letchworth Garden City estate, it is well placed to assist and support various

improvements, some of which are included within this table and are of wider benefit to the Town. We also hope to work with North Hertfordshire Homes who are responsible for a large proportion of the Grange Estate, to promote and deliver projects of benefit to the occupiers of this Estate.

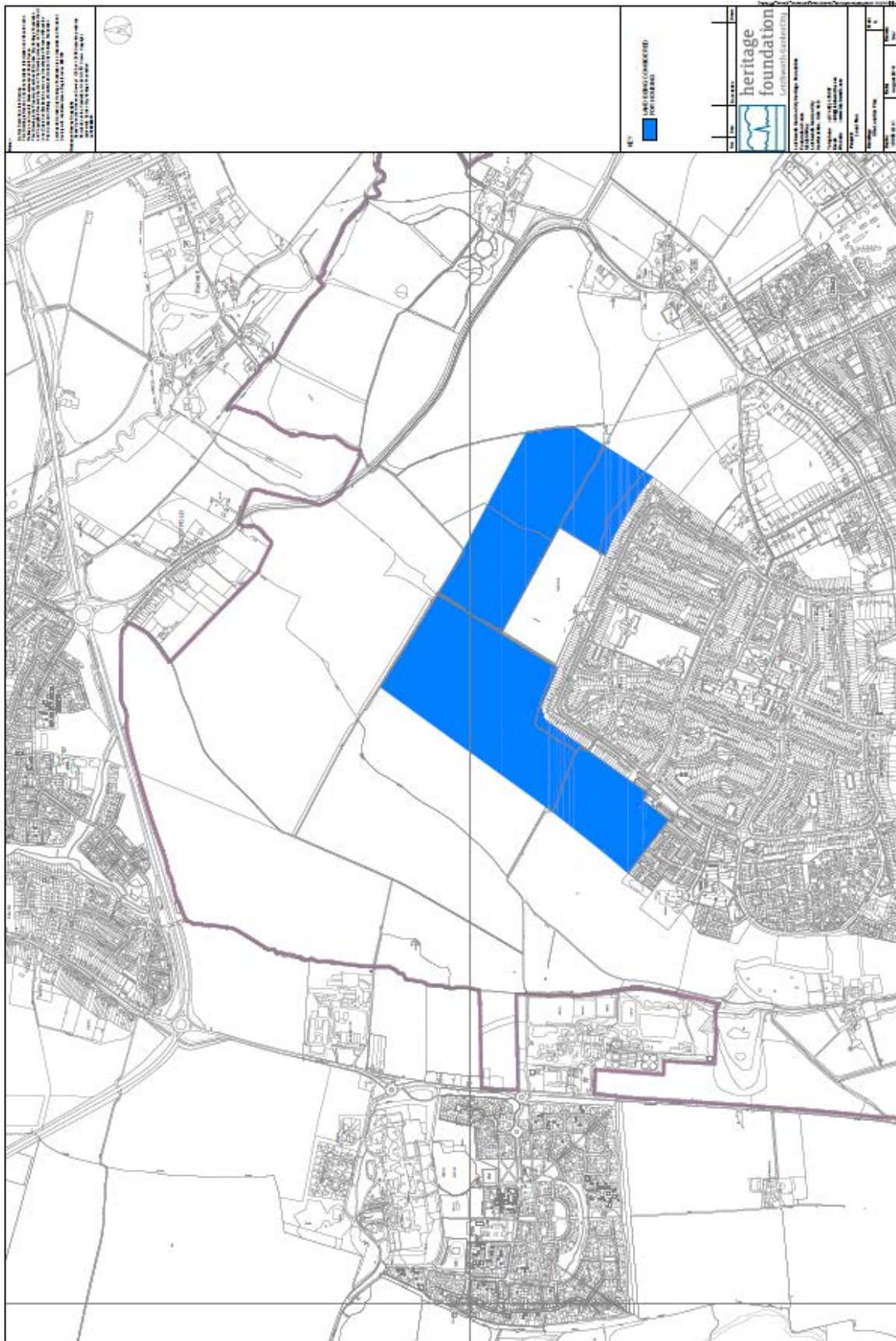
6.0 NEXT STEPS

- 6.1 LGCHF is working on a programme leading to a hybrid planning application, which will be in outline form, other than the first phase of development, which will be submitted in detail. This is expected to be in mid-2020 and will be put together with an appointed partner. This will then lead to a programme facilitating the first delivery of new homes in 2023. The application will follow continued development of the technical analysis of the site, a design competition and in depth community engagement.
- 6.2 The programme leading to the submission of this Planning Application is provided at **Appendix J** to this Statement, which also includes the process leading to the appointment of a development partner.
- 6.3 As part of this exercise, LGCHF's Trustees have already agreed core values and principles linked to the development of the site provided at **Appendix K**. At **Appendix L** is a Communications Plan, which sets out our approach to ensuring a high level of quality engagement with all of the local community who wish to become involved as well as key stakeholders.
- 6.4 A development partner has not been appointed at this stage, as we believe it to be of importance that the Plan should be further progressed before making such an appointment. However, it should be emphasised that LGCHF will be retaining the freehold of the land and working in partnership with an appropriate organisation who shares our values and objectives for the delivery of this site. Our continued involvement in the design, delivery and stewardship of the development will ensure that the site becomes an integral part of the Garden City with strong links to the remainder of the town, making a positive contribution for the benefit of the whole of the local community.
- 6.5 We look forward to developing a detailed Master Plan as part of our community engagement process and working with an appropriate partner to bring the vision of the Board of Trustees forward.

7.0 CONCLUSIONS

- 7.1 LGCHF is committed to the delivery of Strategic Allocation LG1 in accordance with Policy SP15 of the Approved North Herts District Council Local Plan.
- 7.2 We have undertaken an initial analysis of the site and found that there are no significant barriers to delivery and we look forward to working with the local community and our technical team, along with the relevant local authorities, to ensure that the development can come forward in the most sympathetic and beneficial manner.
- 7.3 This will include detailed analysis of the various highways and transportation options, a Master Plan which respects the local ecological attributes of this land and an approach which ensures a high quality sustainable development, reflective of a modern interpretation of Letchworth Garden City Design Principles.
- 7.4 Therefore, the site is available and deliverable. Development will not only meet stated need for housing in Letchworth Garden City, but also create the opportunity for an exemplar development that reflects the strongest aspects of Garden City design, whilst meeting modern sustainable living and place making needs.

APPENDIX A – SITE LOCATION PLAN



PLANNING SUMMARY STATEMENT – SITE LG1 (LAND TO THE NORTH OF LETCWORTH GC)

APPENDIX B – INITIAL INDICATIVE MASTER PLAN



APPENDIX C – TRANSPORTATION STRATEGY SUMMARY (STEER DAVIES GLEAVE)**Technical Note**

To	Letchworth Heritage Foundation (LHF)		
Cc			
From	Steer Davies Gleave		
Date	October 2017		
Project	Land to the North of Letchworth Garden City	Project No.	23045801

Local Plan Site Allocation- Transport Statement**Introduction**

1. Steer Davies Gleave has been commissioned by Letchworth Heritage Foundation (LHF) to provide transport planning and highways advice for the proposed development of the Land to the North of Letchworth Garden City, North Hertfordshire.
2. This note provides an overview of transport consideration pertinent to the promotion of the principle of development on the northern edge of Letchworth to provide more than 1000 new homes. The note has been prepared by Steer Davies Gleave following review of a range of transport studies pertinent to the proposals, local knowledge gained from our involvement with project since 2016 and following discussions with Hertfordshire County Council (HCC) and North Hertfordshire District Council (NHDC).

Baseline Conditions**Location**

3. The identified land is bound by the Grange Estate to the south, and undeveloped land to the north, east and west. Existing cycle and pedestrian routes connect the Grange Estate to Fairfields. The sites location is shown in Appendix A.

Existing Mode Share

4. 2011 Journey to Work Census data for Letchworth Garden City provides an indication of existing mode share within Letchworth as set out in Table 1. It can be expected that the proposed development to the north of Letchworth Garden City is likely to uplift the number of trips made within Letchworth by all modes of travel, particularly increasing demand on the road network, most notably within the Grange Estate.

Table 1: Existing Peak Hour Mode Share – Letchworth Garden City

Mode	Mode Split (%)
Car driver	70%
Car passenger	6%
Motorcycle	1%
Bus	4%
Train	6%
Taxi	0%
Bicycle	3%
On foot	10%



Total	100%
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5. Table 1 illustrates that car dependency is high and that appropriate highway capacity is required to withstand uplift in vehicle trips. However, initiatives being promoted locally and at a strategic level are delivering a shift towards sustainable transport modes. To minimise the impact of the proposals the development will be brought forward along with specific initiatives and investment to develop additional bus services and to encourage pedestrian and cycle movement in and around Letchworth Garden City as a whole.

Local Highway Network

6. The Grange Estate is the predominant road network that will provide connections to the south of the development site. New linkages to the estate will connect with Northfields and Southfields, the major roads within the Estate which are served by local bus routes.
7. The Grange Estate connects to Wilbury Road to the south, an east west route connecting to Norton Road in the east, which continues northbound into Norton village and comprises the northern border to the development site. To the west, Wilbury Road connects with Arlesley New Road, providing an east-west route to Lower Green Village, and Stofold Road, a north-south road connecting to Hitchin Road and Fairfield village and Stofold to the north, and the A505 south-west north-east route to the south, from Walsworth town to the west connecting through Letchworth Garden City to the east.
8. Gaunts Way is the road to the north of the Grange Estate, in close proximity to the proposed development. Roadway widths typically range between 5.0m and 5.1m on residential routes. Northfield and Southfield roads increase in roadway capacity at 7.5m in width, suitable for buses.
9. The variations in road width have been taken into considerations to develop a proposed access strategy to connect the proposed development at the Land to the North of Letchworth Garden City.

Public Transport

10. Letchworth Garden City benefits from good connections to the local bus and rail network. Services and facilities are explored in detail below.
11. Letchworth Garden City is well connected to the national rail network. Letchworth Garden City rail station is located within the centre of the Garden City, approximately 1.9km (24 minutes' walk) south of the proposed development. Letchworth Garden City is served by the Great Northern rail line, with direct connections to Stevenage, Hertford, Finsbury Park and London Kings Cross to the south, and Baldock and Cambridge to the North.
12. Table 2 outlines the key rail services by frequency per hour.

Table 2: Route and Frequency of Rail services at Letchworth Garden City – AM Peak Period (08:00 – 09:00)

Route	Peak hour Frequency (08:00 – 09:00)
Royston – Letchworth Garden City – London Kings Cross	1
Cambridge – Letchworth Garden City – London Kings Cross	3
London King's Cross – Letchworth Garden City - Cambridge	2
Kings Lynn – Letchworth Garden City – London Kings Cross	2
Total	8



13. On average, eight trains serve Letchworth Garden City per hour based on AM peak hour analysis (08:00 – 09:00). A site visit was undertaken on Tuesday 6 December 2016 and high-level rail service frequency counts were undertaken. Between 07:30 and 08:30 AM, a maximum of five services were available until 08:00. There was no recommencement of services until 08:27. This illustrates that within the peak hour (08:00 – 09:00), the majority of services occur after 08:27.
14. Twelve bus services are available within Letchworth Garden City, operated by bus company Intalink as shown in Table 3.

Table 0: Day Bus Services by Frequency per Hour – AM Peak Period (08:00 – 09:00)

Bus Route Number	Route	Frequency per hour
53	Letchworth – Lordship Estate (circular)	3
55	Letchworth – Stevenage	3
90	Royston/Ashwell – Letchworth*	1
91	Royston/Ashwell – Letchworth	2
95	Biggleswade – Letchworth**	1
96	Letchworth – Arlesley/Stotfold – Letchworth	2
96A	Letchworth – Arlesley/Stotfold – Letchworth*	2
97	Arlesley/Stotfold/Baldock – Letchworth – Hitchin*	2
98	Arlesley/Stotfold/Baldock – Letchworth – Hitchin	1
386	Bishop’s Stortford – Letchworth – Stevenage*	1
787	Cambridge – Letchworth – Heathrow Airport	1
W7	Shefford – Letchworth/Baldock***	1
Total		22

*Does not operate during the AM peak period (08:00 – 09:00).

** Tuesdays and Fridays only

*** Fridays only

15. Bus route 55 currently serves the Grange Estate providing three buses per hour.
16. The Hertfordshire Local Plan states that it will:

“Encourage existing car users to change to cycling, walking, passenger transport and car sharing to reduce the proportion of journeys made by car.”
17. To this end, the development offers an opportunity to introduce a new bus link connecting the development to key attractions within Letchworth, such as the train station and town centre. Successful implementation of an additional and/or complimentary bus routes will help connect the development into the wider transport network helping to deliver on the sustainable aspirations of the local council. It is the aspiration of LHF that any new or extended bus service will locate all residential units within 400m of a bus route.



Sustainable Access

Walking

18. The site is well located to make use of existing footways alongside estate roads to provide for pedestrians access local facilities. Letchworth Garden City centre is approximately 1.9km south (24 minutes' walk) with access to a number of retail stores and local amenities.
19. For leisure based trips, Letchworth Garden City benefits from the Greenway for sustainable activity. The Greenway is a 21.8km circular route which borders the Letchworth, providing access to the countryside without straying too far from the town. The Greenway was developed in 2003 following £1million funding from the Letchworth Heritage Foundation.
20. The Greenway currently connects to the northern border of the Grange Estate, adjacent to the proposed development site. A core principle of the development will be to maintain the existing Greenway that borders The Grange Estate whilst establishing a new section of Greenway on the perimeter of the site thus significantly extending the availability of walking routes to existing and new residents.
21. Norton Common is located one kilometre south of the development site and is also suitable for recreational and leisure activity.

Cycling

22. The local roads within the vicinity of the Grange Estate are considered suitable for cycling.
23. National Cycle Route 12 (NCN 12) currently connects through Letchworth Garden City. The total route is 121 miles in length and will extend from Enfield Lock to Spalding when fully complete (the section between Enfield Lock and Hadley Wood is currently under development).
24. Section 1 of the route (the Great North Way) is between Enfield Lock and Letchworth. It currently commences at Hadley Wood and follows predominantly non-traffic routes northbound through Hatfield and Welwyn Garden City, and minor roads through Stevenage. The route between Stevenage and Letchworth is traffic-free, and some sections of the route join with the Hertfordshire Way cycle route.
25. At Letchworth, the route connects from the south, intersecting the east-west A505, continuing northbound and joining with Broadway towards Letchworth Garden City rail station. The route traverses beyond the rail station, through Norton Common into the Grange Estate. The route extends northwards along Grange Road, Danescroft and Sparhawke, connecting with the Greenway to the north of the Grange Estate and the proposed development site. Currently, the NCN 12 intersects the development site, connecting with Norton Road. Beyond Letchworth NCN 12 continues northbound to St. Neots.
26. The 21.8km Greenway route comprising the circumference of Letchworth Garden City and the southern border of the proposed development, is also considered suitable for cycling.

Proposed Access

27. The development will seek to provide access which maximises connectivity to and from the site whilst minimising the impact on the existing infrastructure and environment. Whilst it is accepted that vehicle travel will form a central part of the modal share within the development, appropriate consideration will be given to access strategies for all modes, including sustainable options such as walking and cycling.
28. A holistic approach to access will be adopted which caters for construction vehicles during construction and residential access once the development is complete.



29. Two accesses to the site are currently proposed, including:
 - Norton Road to the north of the site; and
 - Western Way to the south of the site.
30. Western Way will provide a permanent link providing access during the construction phase and once development is complete.
31. Norton Road will provide access for vehicles during construction. Exploration will take place to examine the feasibility of a permanent link as opposed to a construction vehicle only route. This exploration will focus on the potential benefits to accessibility which can be derived from an additional permanent access route.
32. Providing multiple accesses from the development decreases additional demand on the existing road network within Letchworth, instead focusing that demand on bypassing roads, which are better positioned to accommodate additional demand.
33. There is an awareness of the potential for 'rat-running' as a result of improved access, and steps will be taken to ensure the local highway network can accommodate the uplift in trips. All mitigation measures will be considered, including hard measures such as highway improvements to softer behavioural change actions.
34. Previous detailed traffic generation and highway capacity studies have demonstrated that the existing highway network and key junctions have suitable capacity, or can be simply enhanced, to accommodate predicted traffic generation from the site. The emerging access strategy with a link to the north of the site will minimise the reliance on routes through existing residential development to reach the principal road network. Steer Davies Gleave have engaged with Hertfordshire County Council to discuss the access strategy and are content that there are no highway capacity constraints that would limit the current conceived form or scale of development.
35. The impact of the development on this site was assessed in 2013 as part of site selection process for the Local Plan (Transport Assessment, November 2013, Cottee). This assessment included an analysis of an access into the Grange Estate to the south and Norton Road to the north. The assessment concluded that the design capacity of the road surrounding the site is substantially higher than the predicted traffic flows and that should a development proceed it can be accommodated on the highway network provided mitigation measures to improve localised capacity at certain junctions (Green Lane/Norton Road; Eastholm/Norton Road/Wilbury Road; and Stotfold Road/Wilbury Road/Bedford Road) are undertaken.
36. Additional cycle and pedestrian connections will be provided that will provides routes only available for cyclists and pedestrians to encourage these most sustainable and healthy modes of travel.
37. LHF intend to work collaboratively with HCC and NHDC in the design of the access solution and wider Transport Strategy for the site to minimise impact, maximise benefits and provide mitigation measures when deemed necessary. This work will be supported by a detailed traffic modelling exercise to assess highway impact alongside a review of opportunities and constraints surrounding non-car modes.

Integration

38. Integration will be achieved through creating permeable links connecting the existing town to the proposed development. Sustainable modes, such as walking and cycling, will be promoted to help encourage permeability and integration. Hard design measures, such as cycle lanes and street lighting, will be implemented alongside soft behavioural change measures to promote sustainable transport.



39. As part of any future planning submission a Transport Strategy for the site will be developed to encompass the wider area with particular attention paid to how the development can influence other elements of Letchworth with wider benefits such as improved public transport, pedestrian access, cycling facilities alongside enhanced access to/from the town centres and areas of employment.
40. Alongside the site-specific cycling improvements, exploration will take place of how to improve the overall cycling experience within Letchworth. The extensive cycle parking facilities at the train station, shown in Figure 1, are an illustration of how to promote sustainable transport. As part of this project there will be an exploration of how best to deliver a step-change in the numbers of individuals choosing to make trips in a sustainable manner. An example of this could be introducing street lighting in Norton Common to encourage north-south connectivity.

Figure 1: Cycle parking at Letchworth Train Station



Design

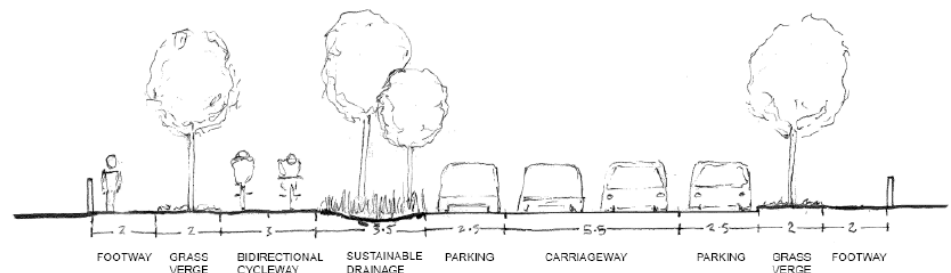
41. The streetscape design will seek to be a contemporary evolution of the Garden City principles, whilst still being firmly rooted in the vision outlined by Ebenezer Howard at the end of the 19th Century.
42. Street typologies will be based on the concept of having a green corridor/spine on the inside of the new development, linking the proposed green spaces and a movement corridor on the outer-side of the new development, with local streets providing residential access. Street parking will be provided in line with local plan guidance but if deemed appropriate may be provided at a lower ratio in places to encourage



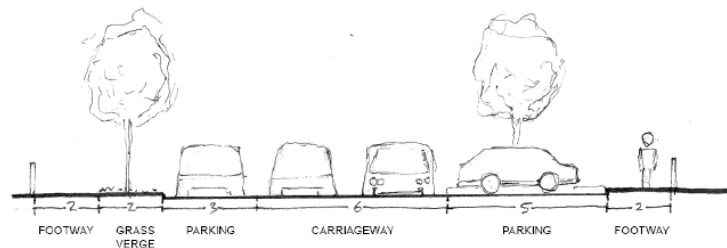
modal shift. There is an aspiration to break up the parking places with sustainable drainage and street trees.

43. The three different proposals shown in Figure 2 reflects the need to provide a variety in the level of connectivity and integration. A lower level of vehicle connectivity allows for an enhanced level of pedestrian and cyclist comfort.

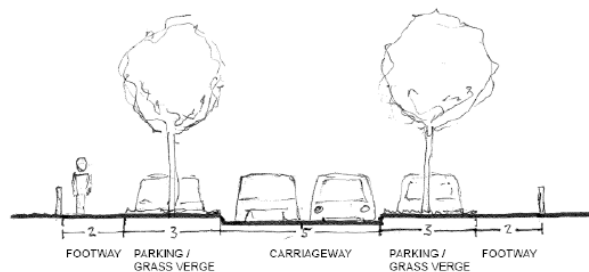
Figure 2: Proposed street scene cross-section



Green Corridor



Movement Corridor



Local Street

Engagement

44. Initial engagement with officers from HCC and NHDC has already taken place and the feedback received through this engagement is helping to shape the proposals which are highlighted above.
45. LHF intend to discuss access and transport issues associated with the site in detail with the wider local community as part of the community engagement strategy. This engagement will shape any future planning submission should this allocation be supported in the Local Plan.

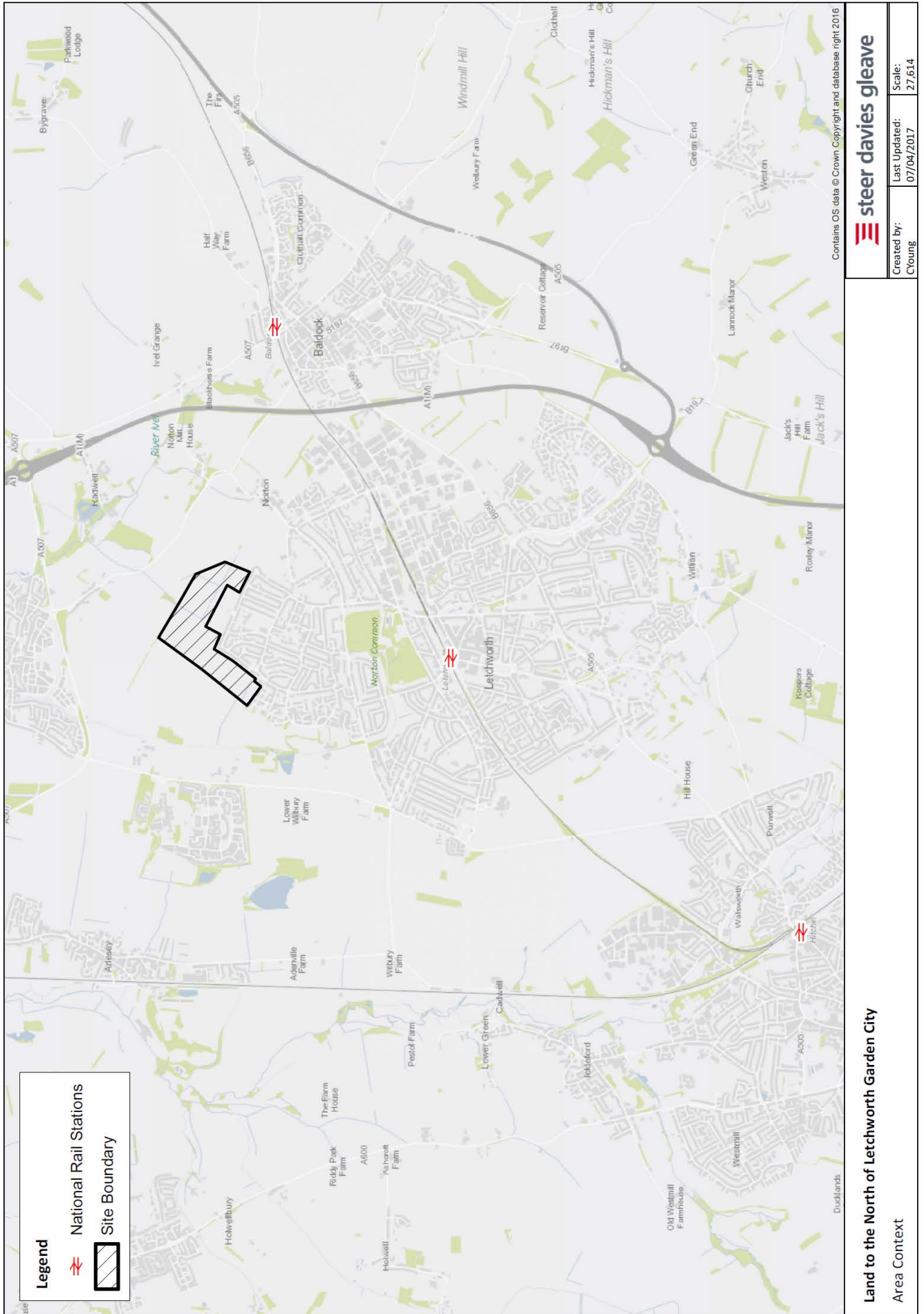


Summary and Conclusions



46. An urban extension to the north of Letchworth Garden City provides an excellent opportunity for bringing forward much needed housing development in a sustainable manner. The site can be integrated with the local highway network and can make use of existing facilities that minimise the need for travel and reliance on the car. It is reasonable to anticipate that the development will facilitate the use of sustainable modes of transport in accordance with paragraph 30 of the National Planning Policy Framework (NPPF).
47. A transport approach and access strategy has been developed and discussed with Hertfordshire County Council and North Hertfordshire District Council. No significant concerns relating to transport have been raised with either the principle of development or emerging details for access and local linkages.
48. The site benefits from good accessibility by road, public transport services and sustainable transport modes:
 - Letchworth Garden City rail station is located within 1.9km of the development site with rail services to London Kings Cross, Stevenage and Cambridge.
 - Twelve daily bus services operate within Letchworth Garden City. Bus service 55 serves the Grange Estate, located immediately south of the proposed development site. Six bus services operate during the AM peak hour (08:00 – 09:00) and PM peak hour (17:00 – 18:00). Connecting to Letchworth Garden City centre and Stevenage.
 - The Greenway is a walking and cycling route which comprises a 21.8km route around the circumference of Letchworth Garden City.
 - Roads within the site vicinity are suitable for cycling. National Cycle Route 12 'the Great North Way' connects through Letchworth Garden City and through the proposed development site connecting from Gaunt Way on the Grange Estate to Norton Road in the north.
49. Highway access can be formed through connections to the existing Grange Estate and to Norton Road providing linkage towards town centre and local facilities and more directly to the Principal Highway Network.
50. Enhanced cycle and pedestrian linkages are being considered for promotion with the development that would provide facilities to encourage the most sustainable and healthy forms of travel. Such facilities would also benefit existing residents.
51. Detailed analysis has shown that existing highway infrastructure can accommodate future traffic patterns with minimal modification. A thorough Transport Assessment will be prepared and further analysis provided in support of the planning applications in due course. The analysis to date provides comfort that the opportunities for sustainable transport modes will be taken up to reduce the need for major transport infrastructure and that safe and suitable access to the site can be achieved for all people.
52. Any necessary improvements can be undertaken within the transport network that cost effectively limit the significant impacts of the development.
53. Paragraph 32 on NPPF states that development should only be prevented or refused on transport grounds where the residual cumulative impacts of development are severe. On this basis, there appears no reasonable grounds for transport objections to the development to emerge.



APPENDIX A – DEVELOPMENT SITE LOCATION PLAN



Legend

-  National Rail Stations
-  Site Boundary

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Created by:	CYoung
Last Updated:	07/04/2017
Scale:	27 614

Land to the North of Letchworth Garden City

Area Context

APPENDIX D – STATEMENT FROM ANGLIAN WATER**North Hertfordshire Local Plan
Letchworth Garden City**

Anglian Water is the sewerage undertaker for the northern part of North Hertfordshire District Council area.

As stated in point 4.5 of the Statement of Common Ground between North Hertfordshire District Council and Anglian Water Services Ltd, it is agreed that there are no outstanding issues in relation to wastewater infrastructure in the area of North Hertfordshire served by Anglian Water Services Ltd (in addition to the changes set out in the schedule of minor modifications). It can therefore be agreed that the plan constitutes sustainable development in accordance with paragraph 180 of the National Planning Policy Framework, March 2012.

As identified in policy SP8: Housing, there is a proposed allocation of 900 homes at North Letchworth, site reference LG1. This site is expected to require upgrades to the foul sewerage network to connect to Letchworth Water Recycling Centre. The timing of these improvements will be agreed with the developer and secured through appropriate provisions in the Water Industry Act 1991. The scale of the upgrades is not expected to impact on the overall delivery of the site and the proposed allocation is therefore consistent with NPPF para 177 which requires that there is reasonable prospect that infrastructure is deliverable in a timely fashion.

Anglian Water is also generally supportive of Policy SP7 as it states that planning permission will only be granted if it can be demonstrated that development proposals make provision for infrastructure that is necessary to accommodate the additional demands resulting from the development.

In general, water recycling centre upgrades where required to provide for additional growth are wholly funded by Anglian Water through our Asset Management Plan. Where we have identified a need for further investment as part of our current business plan we are currently working to identify to what extent improvements are required to serve additional growth in the relevant catchment and how will ensure that we continue to protect the water environment.

Foul network improvements are generally funded/part funded through developer contribution via the relevant sections of the Water Industry Act 1991. The cost and extent of the required network improvement are investigated and determined when we are approached by a developer and an appraisal is carried out. Similarly water infrastructure provision will be dependant on location and

PLANNING SUMMARY STATEMENT – SITE LG1 (LAND TO THE NORTH OF LETCWORTH GC)

scale of the development and contributions for upgrades or strategic schemes will be obtained through provisions in the Water Industry Act 1991. Anglian Water will work closely with the district Council to ensure that the water recycling infrastructure is made available at the correct time.

APPENDIX E – ECOLOGY SUMMARY – RICARDO CONSULTING



Letchworth Garden City Heritage
Foundation

Housing Development: Executive Summary Report

Report for Letchworth Garden City Heritage Foundation
ED11022100

ED 11022100 | Issue Number 1 | Date 20/10/2017
Ricardo in Confidence

Ricardo in Confidence

Ref: Ricardo/ED11022100/Issue Number 1

1 Preliminary Ecological Appraisal

Ricardo (formally Cascade Consulting) was commissioned in December 2016 by Letchworth Garden City Heritage Foundation (LGCHF) to provide an update to a previous Preliminary Ecological Appraisal (PEA) technical report¹ for the site (**Appendix 1 - Figure 1.1**). Within this report a 2km desk study was undertaken using publicly available resources and data procured from the Hertfordshire Environmental Records Centre (HERC). More recent records were requested in October 2016 from HERC and from Bedfordshire and Luton Biodiversity Recording & Monitoring Centre (BLBRMC) to identify the presence of statutory and non-statutory designated nature conservation sites, important habitat features in the study area and the presence and potential distribution of legally protected and ecologically significant species.

The site was visited by two qualified and experienced Ecologists on 27 October 2016. Habitats within and immediately adjacent to the proposed scheme footprint were recorded and mapped using the standard Phase 1 Habitat Survey methodology² and the potential for protected or otherwise notable species was assessed. From this, a number of Phase 2 surveys were recommended and undertaken, as summarised below.

2 Results and Further Surveys

2.1 Designated Sites & Notable Habitats

There are no European Sites, Sites of Special Scientific Interest (SSSI) or National Nature Reserve (NNR) statutory designated nature conservation sites within 2km of the proposed site. The HERC report identified 14 non-statutory local-level wildlife sites within 2km of the development boundary, however these are not anticipated to represent significant constraints to development.

The Phase 1 Habitat Survey identified the following key habitats: arable; species-poor intact hedgerows; broadleaved plantation woodland; mixed plantation woodland; dry ditches; scattered trees; scattered scrub; and tall ruderal vegetation (**Appendix 1 - Figure 1.2**).

Early consideration in the design phase can ensure that hedgerow loss is kept to a minimum and as many hedgerows as possible are retained or compensated for in the landscape design of the scheme by planting native species-rich hedgerows. Similarly, all trees should be retained wherever possible and new native tree planting included in the landscape design.

2.2 Great Crested Newts (GCN) (*Triturus cristatus*)

The 2016 desk-studies identified the presence of GCN and common toad (*Bufo bufo*) within 2km of the site. Three ponds were also identified within 500m of the site with potential to support GCN (**Appendix 1 - Figure 1.4**). Referring to Policy 4 advice³, the area of GCN habitat within the development site estimated to be permanently lost is <5ha. A GCN presence/ likely absence survey was attempted at Ponds 1 and 3. It was not necessary to perform survey of Pond 2 as GCN were already known to be present⁴. Pond 2 is located 380m south-east of the development site. Clearance works should proceed with caution and should be undertaken under a Precautionary Method of Working (PMW). This may include controlled removal of vegetation, through strimming and hand searching by a suitable qualified ecologist. If GCN are found works must cease and an EPS mitigation licence applied for.

Pond 1, located on the secure Anglian Water Sewerage Works site, contained thick reeds (meaning access to much of the pond was not possible) and was separated from the site by a steep sided ditch, with intervening sub-optimal habitat (arable and residential). At a significant distance (435m) from site, it is highly unlikely to act as a source of GCN risk. As a result, no further surveys were conducted. Pond 3 is located in the back garden of a residential property, to which the surveyors did not have access permission at the time of survey, therefore this pond still requires an eDNA assessment. However, even if GCN are present here the precautionary clearance method described above will mitigate the risk of significant impact.

¹ Cascade Consulting (4 November 2013) Housing Development Technical Report – Ecology, Letchworth Garden City Heritage Foundation

² JNCC, (2010), Handbook for Phase 1 habitat survey - a technique for environmental audit, ISBN 0 86139 636 7

³ GCN licence application Method Statement Advice

⁴ Maydencroft (September 2016) Norton Pond Restoration Plan, Letchworth Garden City Heritage Foundation

Any toads encountered during site clearance should be captured by hand, placed in a bucket and immediately released in to suitable retained habitat (for example, rough grassland, bases of hedgerows, woodland) ideally somewhere shady and slightly damp.

2.3 Bats

The 2016 HERC records include the brown long-eared bat (*Plecotus auritus*), common pipistrelle (*Pipistrellus pipistrellus*), Daubenton's bat (*Myotis daubentonii*), and serotine (*Eptesicus serotinus*).

The update PEA survey identified four mature trees (TN1-4) on the north-western boundary of the site with holes, split limbs and loose bark. It is recommended that all existing trees, woodland blocks and hedgerows are retained wherever possible but if this cannot be accommodated, a detailed climbed inspection survey of identified tree features should be undertaken in March 2018 in order to identify the presence of any roosting bats or their field signs⁵. If bats are found to roost in any tree to be impacted, a European Protected Species (EPS) mitigation licence will be required.

Where hedgerows are permanently severed by the development, a suitable, fast-growing, native tree species should be planted at either end of the gap which in time could help to bridge the gap via the canopy to restore bat commuting routes. Other mitigation opportunities include the provision of bat boxes on suitable trees and incorporated into building structures.

2.4 Hazel Dormouse (*Muscardinus avellanarius*)

No records of hazel dormice were returned, although they are known to exist in the county. Therefore, the development design should seek to retain all linear woody corridors, such as hedgerows and woodland blocks. Wherever possible, new linear features should link to and enhance the existing field boundary features, including a mix of native hedgerow shrubs and trees. Where full habitat retention is not possible, surveying for dormice is required⁶, to be set up in March 2018 and conducted from May to October 2018. These surveys will inform appropriate mitigation which may require an EPS mitigation licence. If the area to be cleared is considered to be small, this should be undertaken following a non-licensed dormouse method statement rather than an EPS mitigation licence.

2.5 Nesting Birds

Biological records included a total of 74 bird species within 2km of the site including 13 NERC S41 species, 8 WCA Schedule 1 species, 39 species of conservation concern identified on the Amber list and 23 species identified on Red list in the UK.

All habitats within the site have potential to accommodate breeding birds during the nesting season (March-August inclusive), therefore all woody vegetation clearance should be undertaken outside of the bird nesting season. Where this is not possible, a bird nest check will need to be undertaken within 48 hours prior to any proposed clearance. If an active nest is recorded, then a temporary 5m exclusion zone must be established around the nest and remain in place until all fledglings have left the nest.

2.6 Reptiles

Biological records include common lizard *Lacerta vivipara*, grass snake *Natrix natrix* and slow worm *Anguis fragilis*. Although none of these fell within the proposed development site, the majority of habitats within the study area were considered suitable to support reptiles, therefore a presence/ likely absence survey was conducted between July and August 2017 in suitable weather conditions. No reptiles were recorded, concluding that reptile status within the survey area is 'likely absent'.

In the unlikely event of reptiles being encountered, works in that area should immediately cease and an experienced ecologist should be contacted for advice to avoid injuring or killing any animals.

The development design should incorporate areas of rough, unmanaged grassland and scrub to provide habitats for reptiles. Therefore, the retention of existing grass margins and scrub areas would be beneficial. Wherever possible, further refuge habitat could be created by providing log and/or rubble piles.

⁵ 'Bat Surveys – Good Practice Guidelines 3rd Edition, BCT' (2016).

⁶ English Nature (2006) The dormouse conservation handbook. Second edition

2.7 Badgers (*Meles meles*)

The biological records included five records of dead badgers found on a road, within 2km of the site. A dedicated survey was undertaken on 9 May 2017 but no signs of badger occupation were recorded on site. A hedgehog carcass, *Erinaceus europaeus*, was found on the site periphery (TL2209 3515) which appeared to have been eaten in a manner characteristic of badgers, suggesting they are likely to be present in the surrounding area but don't currently have setts on site. No impacts on badgers are currently predicted, however pre-construction checks will be required if 12 months elapses after the survey.

2.8 Water voles (*Arvicola amphibius*) and Otters (*Lutra lutra*)

Biological records included several water vole records, the most recent from 2007 in the River Ivel and Hiz County Wildlife Site. The report also included several records of otters within 2km of the site. All ditches on the site were dry when visited in October 2016. The dry ditch adjacent to the eastern boundary of the site did not contain any riparian vegetation and was assessed as being highly unlikely to support water voles or otters.

2.9 White clawed crayfish (*Austropotamobius pallipes*)

No records of white-clawed crayfish were returned and there were no suitable habitats on or immediately adjacent to the site that could support this species. Pix Brook lies approximately 400m from the SE boundary. Subject to the location of an access route to the site, this feature is sufficiently distant to be unaffected by these proposals.

2.10 Brown hare (*Lepus europaeus*), European hedgehog (*Erinaceus europaeus*) and harvest mouse (*Micromys minutus*)

Impacts on brown hare, hedgehog and harvest mouse populations are unlikely to be significant. A watching brief for these species should be maintained during site clearance, and if found an ecologist should be contacted for advice.

Appendix 1

Figure 1.1 Study Area and Development Boundary (Source: Letchworth Garden City Heritage Foundation PEA appraisal Report 2013 Cascade)



Figure 1.2 Phase 1 Habitat Survey (Source: Letchworth Garden City Heritage Foundation PEA appraisal Report 2017, Ricardo)

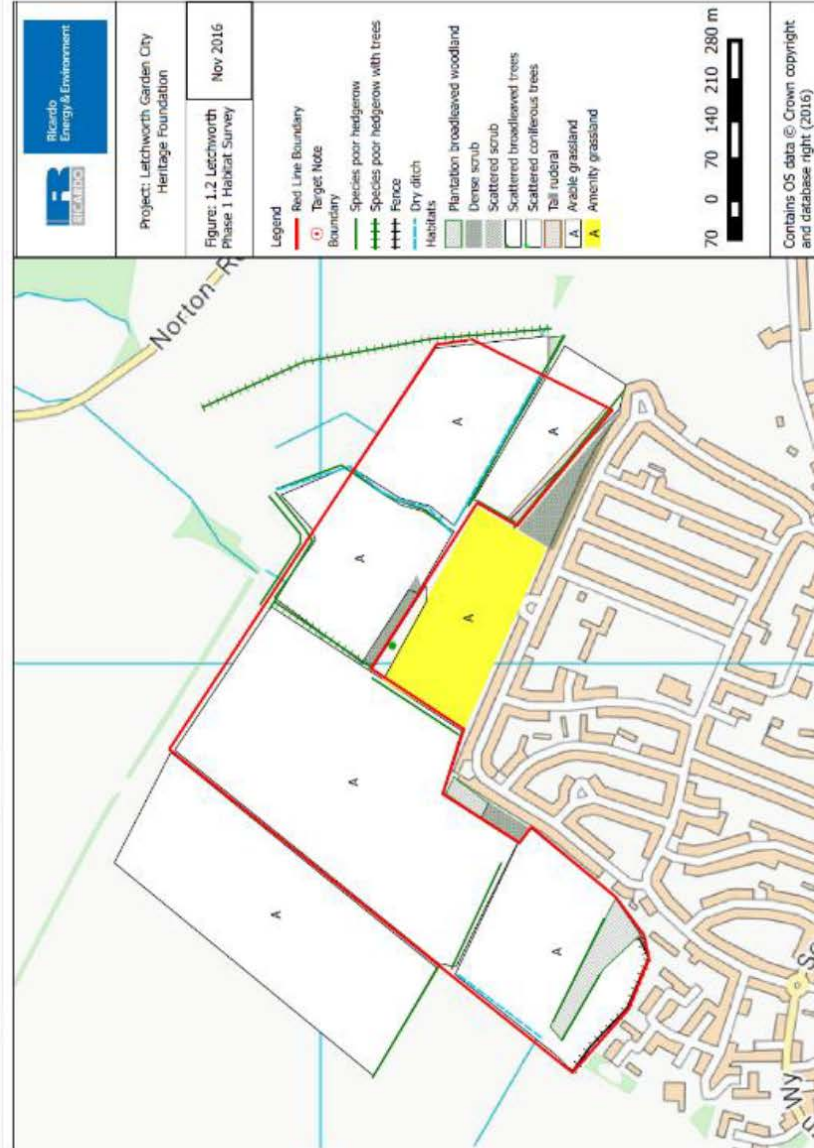
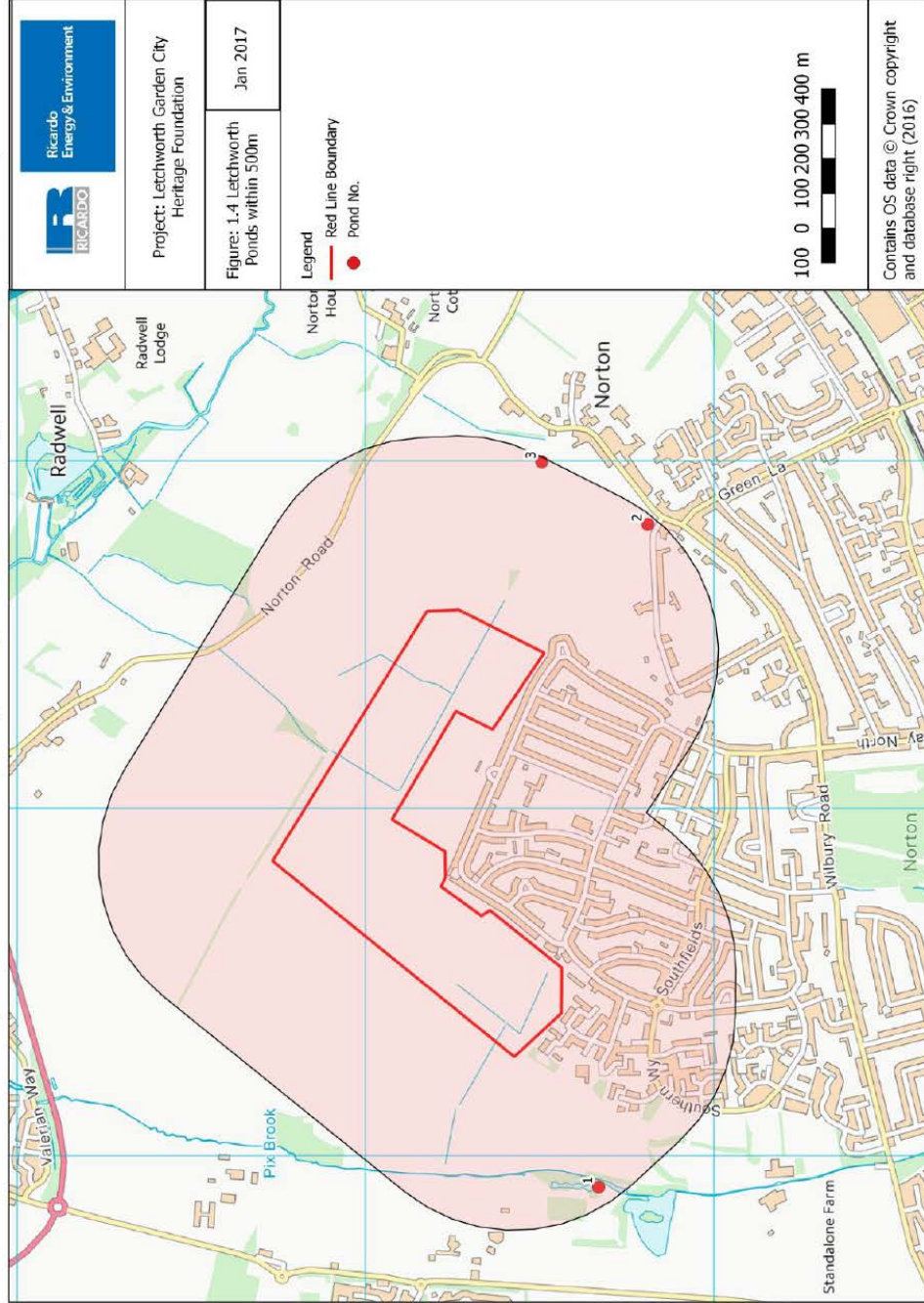


Figure 1.4 Letchworth Ponds within 500m (Source: Letchworth Garden City Heritage Foundation PEA Appraisal Report 2017, Ricardo)





Ricardo
Energy & Environment

The Gemini Building
Fermi Avenue
Harwell
Didcot
Oxfordshire
OX11 0QR
United Kingdom

t: +44 (0)1235 753000
e: enquiry@ricardo.com

ee.ricardo.com

APPENDIX F – HYDROLOGY & FLOOD RISK – RICARDO CONSULTING



Letchworth Garden City Heritage Foundation

Housing Development: Executive Summary Report – Hydrology and Flood Risk Assessment

Report for Letchworth Garden City Heritage Foundation
ED11022100

ED 11022100 | Issue Number 1 | Date 20/10/2017
Ricardo in Confidence

Executive Summary

The hydrology and flood risk technical appraisal report is required to provide the Letchworth Garden City Heritage Foundation Board with information on the constraints and opportunities associated with the potential development site to enable them to make a decision on the site and to provide information to the local community.

There is a 'very low risk' of flooding from rivers (fluvial) and a low risk from groundwater and surface water (rainfall runoff) to the development site, which will need to be considered further at the detailed design stages. There will be a significant increase in the volume and amount of rainwater runoff from the developed site due to increased areas of hard surfaces.

New evidence has been documented from reports of two flood incidents in the surrounding area to the development site, how these did not impact the immediate area of the site. These included the flood incident at Pix Brook, Stotfold, in July 2015 and the flood incident in the main urban areas of Letchworth Garden City, in June 2016. The above flood incidents highlighted the vulnerability of the River Ivel catchment and urban areas of Letchworth Garden City to flash flooding following a significant rainfall event.

The SuDS viability plan in the North Hertfordshire District Council Strategic Flood Risk Assessment shows that the geology/soils of this site suggest that it is not suitable for infiltration-based SuDS techniques. Where infiltration-based SuDS techniques are not suitable, then attenuation-based SuDS techniques should be considered instead.

With consideration of the recent flood incidents and given the proximity of the development site to Pix Brook, the main constraint on development at this site in relation to hydrology and flood risk will be surface water management. Therefore, a surface water management strategy will be needed to prevent flooding further downstream. The strategy should focus on using sustainable urban drainage methods to ensure that the amount and rate of runoff from the developed site is no greater than from the existing Greenfield site, and may include storage ponds or underground tanks. A full Flood Risk Assessment for the site would be required to support any planning application for the development.

APPENDIX G – ARCHAEOLOGY – ARCHAEOLOGICAL SOLUTIONS

LAND TO THE NORTH OF LETCHWORTH, AREAS 4 & 10. SUMMARY OF ARCHAEOLOGICAL WORK

In October 2016, Archaeological Solutions Ltd (AS) conducted an archaeological desk-based assessment of the proposed development site. This concluded that the site lies in an area of archaeological potential and that it incorporates part of Area of Archaeological Importance No. 119, which partly falls within its easternmost corner and which is associated with undated cropmarks of a rectilinear field system. The archaeological potential of the site was assessed as follows:

Prehistoric –Low to moderate. The undated cropmarks of a rectilinear field system located just beyond the eastern boundary of the site (HER 2488) may represent settlement enclosures possibly dating to the Bronze Age or Iron Age.

Roman – Moderate. A Roman As coin of Hadrian was also found within the western section of the site in 1973 (HER 1254).

Saxon – Low. Limited Anglo-Saxon remains are known in the Norton area surrounding the site.

Medieval – Moderate. A Long Cross penny (pre 1377) was found during 1973 in the eastern section of the site and along the course of a drain (HER 1253).

Post-Medieval – Low. Historic cartographic sources reveal that the site consisted of agricultural land.

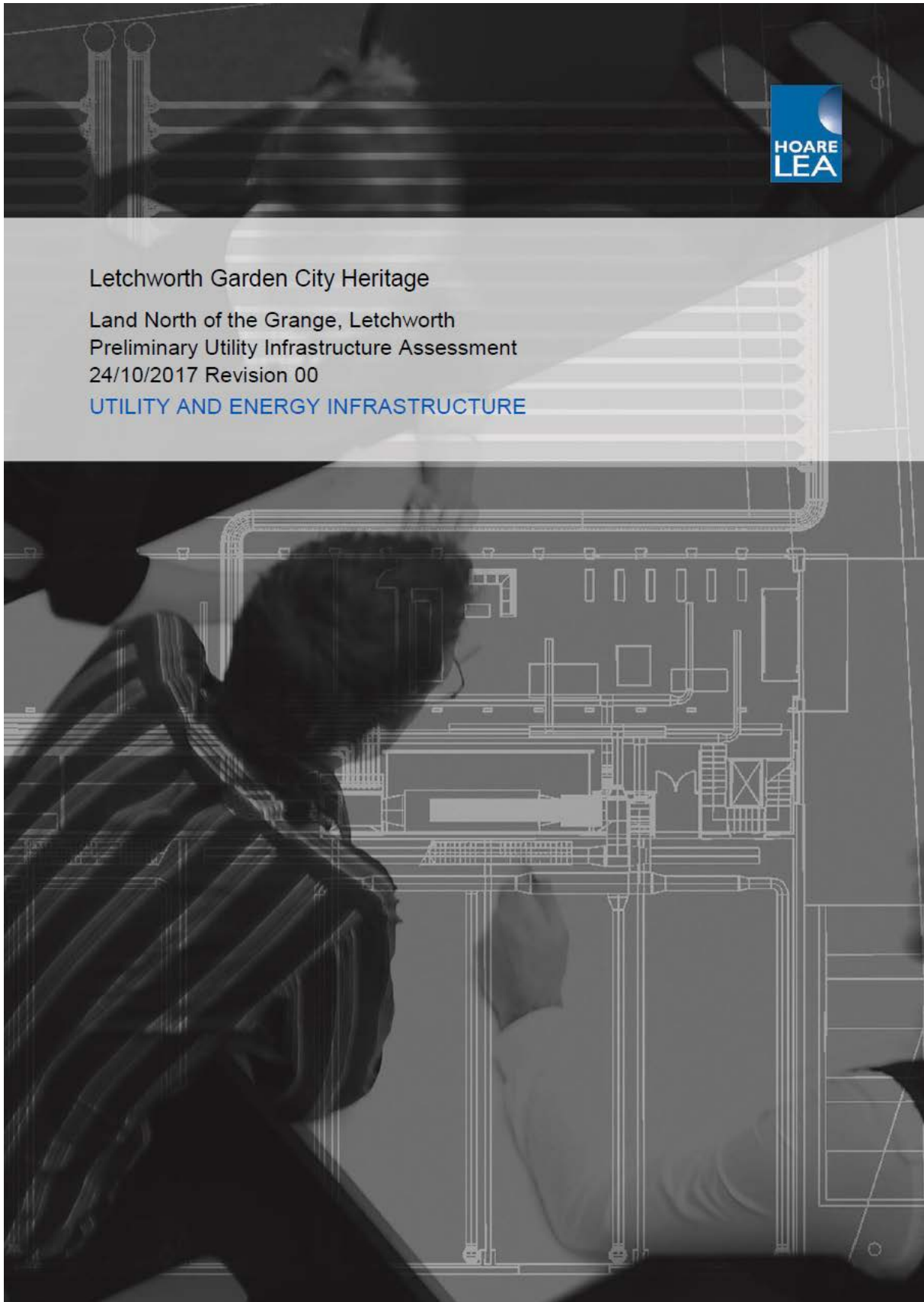
This desk-based assessment was submitted to Andy Instone, Senior Historic Environment Advisor at Hertfordshire County Council who noted that the scale of the proposed development was likely to have an impact on significant heritage assets with archaeological interest and that it would be likely that;

- A geophysical survey over the entire site, where ground conditions permit, and
- An archaeological field evaluation to test the geophysics results and quantify the archaeological resource

Would be required prior to determination of any planning application for the site.

On the 21st June 2017, AS conducted a site visit to ascertain existing ground conditions at the site. It was noted that the entirety of the site was under crop and that it would not be possible to carry out a geophysical survey until the crop had been harvested. A specification and proposal for carrying out such work was provided to Letchworth Garden City Heritage Foundation at this time.

APPENDIX H – UTILITIES REPORT - HOARE LEA



UTILITY AND ENERGY INFRASTRUCTURE
 Land North of the Grange
 Preliminary Utility Infrastructure Assessment



Audit sheet

Rev.	Date	Description	Prepared	Verified
00	24/10/2017	Initial First Draft	PNG	LGJ

This report has been prepared for Letchworth Garden City Heritage only and expressly for the purposes set out in an appointment dated TBC and we owe no duty of care to any third parties in respect of its content. Therefore, unless expressly agreed by us in signed writing, we hereby exclude all liability to third parties, including liability for negligence, save only for liabilities that cannot be so excluded by operation of applicable law.

UTILITY AND ENERGY INFRASTRUCTURE

Land North of the Grange

Preliminary Utility Infrastructure Assessment



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UTILITY AND ENERGY INFRASTRUCTURE

Land North of the Grange

Preliminary Utility Infrastructure Assessment



1. Introduction

HL have been appointed by Letchworth Garden City Heritage to undertake an initial investigation in to the constraints posed by existing utility services and appraise potential risks in obtaining adequate capacity from local infrastructure to support the proposed Land North of the Grange development.

This report summarises a high level assessment of the existing utility services infrastructure surrounding the proposed Land North of the Grange development. Following a desktop study only, from the existing utility record drawings and report pack obtained by Landmark Information Group, this evaluates the impact in accommodating the proposed new development. It highlights the key issues and potential constraints the existing utility infrastructure may pose on the new development. Assessments have been undertaken against obtained record information only, no consultation with the incumbent service providers has been made.

1.1 The site

The appraised site is located to the north of Letchworth, between the A507 to the north, Wilbury Road to the south, Fairfield Park to the west and Norton Road to the east, Hertfordshire SG6 4SY, SG6 4TQ. The site is currently a green field area and unoccupied.

It is understood the proposal is for approximately 900 residential dwellings, provision of a new Primary School, 9 serviced plots for self-build development and approximately 900sqm GIA of A-Class floor space.

Grid Reference – X: 521996 Y: 234811 Easting/Northing



Figure 1 - Proposed Development Site, Location Plan

UTILITY AND ENERGY INFRASTRUCTURE

Land North of the Grange

Preliminary Utility Infrastructure Assessment



REFERENCE: 55405
SITE: Land off Gaunts Way, Letchworth Garden City
POST CODES:
SG6 4SY,SG6 4TQ
SITE SIZE: 75.85 ha
MAP SCALE: 1:15000
COORDINATES:
1) 521996 234811; 2) 522726 234658; 3) 521838 235366; 4) 521252 234521; 5) 521483 234311;

Figure 2 - Proposed Development Site, Location Details

2. Headline Issues

2.1 Existing Infrastructure

Utility services record drawings for the primary utility providers in the area have been obtained and reviewed with respect to impact appraisals. The service providers below have infrastructure that may be impacted by the scheme:

Electricity	UKPN Power Networks (UKPN)
Gas	Cadent Gas Ltd (CGL), [Formerly National Grid Gas]
Water	Affinity Water (AW)
Drainage	Anglian Water Sewerage (AWS)
Telecommunications	BT Openreach (BT) Virgin Media (VM)

2.2 Diversions & Disconnections

The preliminary assessment has identified electricity and gas services within the site boundary, routed through the proposed development, together with existing services distributing through and around the perimeter of the adjacent Grange housing estate.

An existing CGL IP (Intermediate Pressure, of between 2-7 bar) gas main traverses the site, towards the north, laid underground from east to west.

An existing UKPN 33kV EHV (Extra High Voltage) electricity cable, installed both underground and over ground, traverses the site, from east to west.

Affinity Water and Anglian Water Sewerage, potable water and drainage services respectively do not appear to exist within the proposed development site, only existing services distributing through the adjacent Grange housing estate.

A BT Openreach and Virgin Media telecommunications distribution network serves the existing and the adjacent Grange housing estate and do not appear to exist within the proposed development site.

UTILITY AND ENERGY INFRASTRUCTURE

Land North of the Grange

Preliminary Utility Infrastructure Assessment



2.3 New Supplies & Reinforcement

Electricity – UKPN Long Term Development Statement (LTDS) indicates an availability of capacity within the local network primary substations (33/11kV), although both the East and West Letchworth substations are highly utilised. Offsite upgrades may be required at primary substation level (33/11kV). The UKPN distributed generation map indicates the local substations are currently unconstrained with available generation capacity, meaning CHP may not be constrained for export.

Gas – An Intermediate Pressure gas main service exists within the proposed development site running east to west. Low Pressure gas main services exist within the immediate vicinity of the adjacent Grange estate development. Assumed worst case scenario is the requirement for a gas pressure reducing station at the site boundary. Further investigations required.

Water – Application will need to be submitted with Affinity Water to understand current network capabilities, though there is reasonable infrastructure in the adjacent Grange estate development to the south of the development site.

Drainage – Record information indicates the local network generally comprises foul, surface and combined water sewer pipes, within the adjacent Grange estate development, supplying the existing buildings.

Telecoms – Local Telephone Exchange appears to be FTTC (Fibre to the Cabinet) enabled. Thinkbroadband speed test results indicate the area is well serviced with telecom infrastructure, obtaining speeds above the governments superfast broadband requirements (24Mbps). BT Openreach and Virgin Media have infrastructure in the area from which a connection can be sought.

UTILITY AND ENERGY INFRASTRUCTURE

Land North of the Grange

Preliminary Utility Infrastructure Assessment



3. Electricity – UK Power Networks (UKPN)

3.1 Existing Infrastructure

UK Power Networks (UKPN) is the incumbent electricity provider in the area. Record drawings indicate the existing development site does not include any network distribution HV/LV substations. The electrical network services supply the adjacent Grange estate development with both 11kV high voltage (HV) and low voltage (LV) distribution feeds, which do not appear to affect the proposed new development.

Records depict an existing 33kV underground EHV cable in the east of the site, but primarily rising and running overhead to the West Letchworth Primary Substation, traversing the proposed development site from east to west. Any work within the vicinity of overhead power lines will need to be carried out in conjunction with both UKPN's safe working practice and in accordance with HSE recommendations and requirements. As an alternative consideration, in terms of cost and programme, could be given to diverting the overhead cable route and/or alternatively routing the cable below ground.

Please refer to the existing record drawings report pack.

The nearest primary substations (33/11kV) have been identified as East Letchworth and West Letchworth, located as indicated on the Record Mapping layout below.

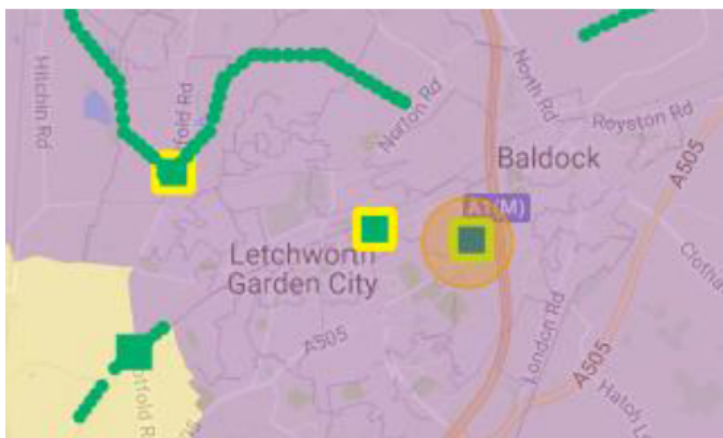


Figure 3 - Extract from UKPN Distributed Generation Mapping Tool

UTILITY AND ENERGY INFRASTRUCTURE

Land North of the Grange

Preliminary Utility Infrastructure Assessment



3.2 New Supplies & Reinforcement

Data obtained from the UKPN Long Term Development Statement (LTDS) 2017 indicates available capacity within the two local 33/11kV Primary Substations.

From an initial load assessment of the required new electrical demand capacity (900 residential units, Primary School and 900sqm of A Class = circa 2.5 – 3.0MVA) it would appear load capacity for the new development may available from the existing primaries without the need for off-site reinforcement.

Table 1 - Data Extract from UKPN LTDS May 2017

Substation	Maximum Demand 15/16		Forecast (Maximum Demand) MW					Firm Capacity	Minimum Load Scaling Factor
	MW	PF	16/17	17/18	18/19	19/20	20/21	MW	%
East Letchworth Primary 11kV	18.00	0.96	18.10	18.20	18.40	18.50	18.70	22.10	29.80%
West Letchworth Primary 11kV	11.90	0.96	12.00	12.10	12.30	12.40	12.60	21.90	17.20%

UTILITY AND ENERGY INFRASTRUCTURE

Land North of the Grange

Preliminary Utility Infrastructure Assessment



4. Gas – Cadent Gas (CG)

4.1 Existing Infrastructure

The local gas infrastructure is owned and maintained by Cadent Gas, (formerly National Grid Gas). The network features an underground Intermediate Pressure (2 – 7bar) pipework gas main traversing the proposed development site to the north and running from east to west.

It should be noted that no excavations will be permitted within an exclusion zone of 3.0 metres from the buried IP gas main service pipe.

Please refer to the existing record drawings report pack for further detail.

4.2 New Supplies & Reinforcement

Given the presence and extent of the Intermediate Pressure gas pipe main within the proposed development site, the availability of gas in the local network is unlikely to constrain the development.

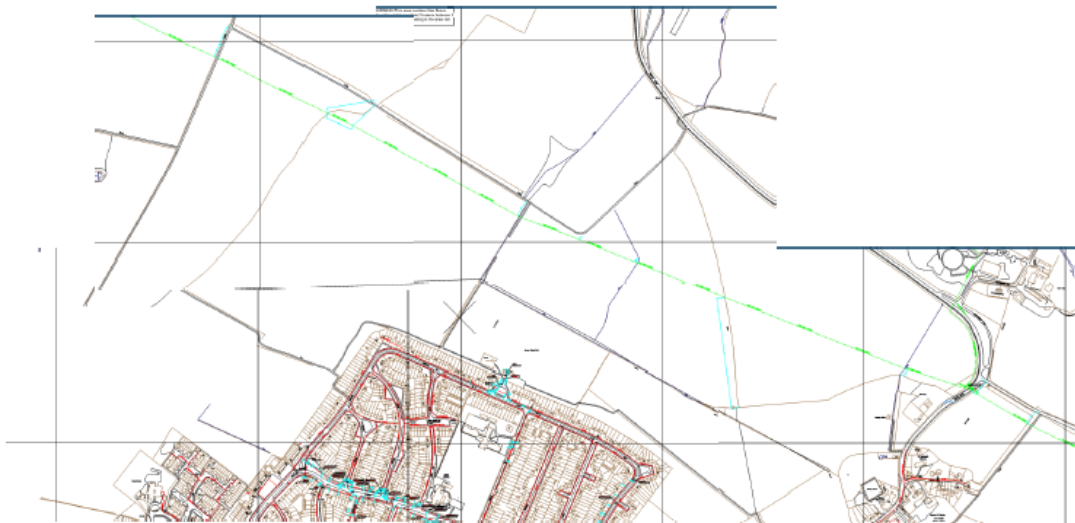


Figure 3 - Extract from Cadent Record Information

UTILITY AND ENERGY INFRASTRUCTURE

Land North of the Grange Preliminary Utility Infrastructure Assessment



5. Potable Water - Affinity Water (AW)

5.1 Existing Infrastructure

The local potable water infrastructure is owned and maintained by Affinity Water (AW). An existing water distribution service does not appear present within the proposed development site. The adjacent Grange estate development is supplied via 4" dia cast iron/spun iron distribution mains.

Please note water service providers do not record the location of privately owned water connections which may be present, details of any existing on-site utilities should come from the land owner.

Existing water mains location and material do not appear to pose any constraint to the proposed development.

Please refer to the existing record drawings report pack.

5.2 New Supplies & Reinforcement

Affinity Water (AW) may request a modelling study to determine the impact the proposed development will pose on the water supply network, detailing reinforcement as necessary to maintain similar levels of service within the local flow monitoring zone. Local upgrades may be required to bring a service into the site boundary, the need for offsite network upgrades will need verification from AW.

6. Sewerage – Anglian Water Sewage (AWS)

6.1 Existing Infrastructure

The sewerage operator for the area is Anglian Water Sewers (AWS). An existing sewer network does not appear to exist within the development site. Local to the site are foul, surface and combined sewer pipes serving the adjacent Grange estate development.

Please refer to the existing record drawings report pack.

6.2 New Supplies & Reinforcement

Further investigations with Anglian Water Sewage are required.

UTILITY AND ENERGY INFRASTRUCTURE

Land North of the Grange Preliminary Utility Infrastructure Assessment



7. Telecommunications – BT Openreach (BT) and Virgin Media (VM)

7.1 Existing Infrastructure

An existing BT Openreach and VM network service exists, running from the existing Telephone Exchange in Nevells Road serving the adjacent Grange estate development with a number of distribution points, cabinets and chambers etc, installed within and around the estate perimeter.

No existing or contentious telecommunications services have been identified within the site boundary.

Please refer to the existing record drawings report pack.

7.2 New Supplies & Reinforcement

BT Openreach and Virgin Media have infrastructure in the area from which a connection can be sought.

The nearest telephone exchange has been identified at Nevells Road. This exchange appears to be fibre enabled.

Thinkbroadband speed test results, pictured below, indicate the area is well serviced with telecom infrastructure obtaining speeds above the governments superfast broadband requirements (24Mbps).

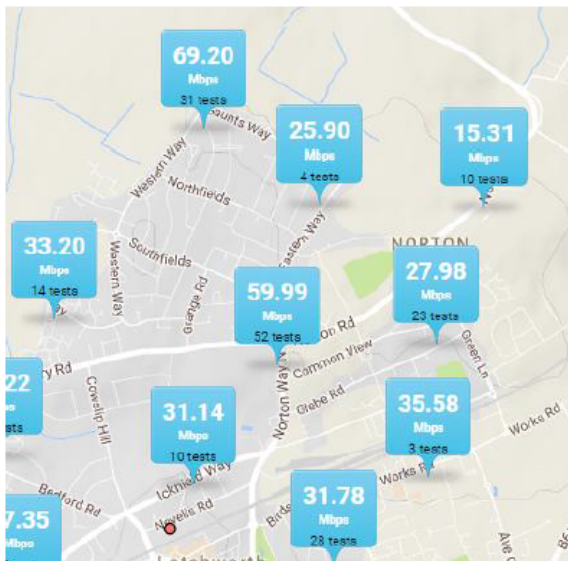


Figure 4 - Speed Test Result (Extract from Thinkbroadband.com)

UTILITY AND ENERGY INFRASTRUCTURE
Land North of the Grange
Preliminary Utility Infrastructure Assessment



End of Document

APPENDIX I – HOUSING ASSOCIATION SUPPORTING LETTERS

Chief Executive's Office



Rowan House
Avenue One
Letchworth Garden City
Hertfordshire SG6 2WW

T 01462 704119
DX 45009 Baldock
www.nhh.org.uk

Private & Confidential

David Ames
Head of Strategic Planning and Development
Letchworth Garden City Heritage Foundation
Foundation House
Icknield Way
Letchworth Garden City
Hertfordshire, SG6 1GD

E-mail: gavin.cansfield@nhh.org.uk

25 October 2017

Dear David

I'm writing to you on behalf of North Hertfordshire Homes, in response to the Local Plan submitted by North Hertfordshire District Council.

We are pleased to see that the Plan includes allocation for a considerable amount of new affordable housing in this area. We would look to work closely with you and colleagues at the Letchworth Garden City Heritage Foundation to deliver these homes.

We note that one of the areas identified for development in the Plan is land to the north of the Grange Estate in Letchworth, and that vehicle access to this route would be through land owned by North Hertfordshire Homes on the Western Way. Without prejudice to any future discussions that might take place, I confirm that in principle we would be willing to engage with yourselves and the community to develop an appropriate solution to enable the delivery of these much-needed new homes for the town.

We look forward to hearing from you as the Local Plan progresses.

Yours sincerely



Gavin Cansfield
Chief Executive



Chief Executive: Gavin Cansfield

North Hertfordshire Homes Limited, Registered office:
Rowan House, Avenue One, Letchworth Garden City, Hertfordshire SG6 2WW
Registered Provider with the Homes and Communities Agency (Reg No L4370).
Registered with the Financial Conduct Authority under the Co-operative and
Community Benefit Societies Act 2014 (Reg No 30003R) and an exempt charity.

Howard Cottage Housing Association
 Pioneer House, Norton Way South,
 Letchworth Garden City, Hertfordshire SG6 1NY

► 01462 683307 ► enquiries@howard-cottage.co.uk ► www.howard-cottage.co.uk



18th October 2017

Mr David Ames,
 Letchworth Garden City Heritage Foundation,
 Foundation House,
 Icknield Way,
 Letchworth Garden City
 SG6 1GD

Letchworth Garden City
 Heritage Foundation

19 OCT 2017

Received

Dear David

Land North of the Grange – Local Plan Submission

I am writing in support of the proposals that the land identified for housing north of the Grange Estate in Letchworth Garden City is allocated for future housing.

As you know Howard Cottage have been supportive of the scheme since the initial proposal some time back. There is no doubt the country is in the midst of a housing crisis and housing of all tenures is required, but particularly really affordable rented housing has never been in greater demand. This is now being recognised by central government with the recent announcement of greater funding provision in this tenure – for the first time since 2010.

From our own point of view we have residents who aspire to move to more suitable accommodation and many young local people who are being forced to move away from the town due to a lack of affordable housing. Whilst we aspire to develop the homes they need we have found it increasingly difficult to do so due to the lack of affordable land where planning permission is likely. When this is found we are inevitably out priced by private developers.

We see the land identified here as a golden opportunity to provide a real asset to Letchworth in its second century. The chance to develop an extension to the Garden City under the principles of the Garden City model itself is truly unique. Letchworth was originally designed to provide a home for all people where they could live and work. Howard Cottage has been part of this development of the town since 1911 and would welcome the opportunity to work with the Heritage Foundation to continue to do that on this new enhancement to the town

Yours sincerely

A handwritten signature in black ink, appearing to read 'John Welch', with a long horizontal flourish extending to the right.

John Welch
 Chief Executive

A charitable housing association registered under the
 Industrial Et Provident Societies Act Reg No 5305R
 Housing Corporation Registration No. L1312



business for neighbourhoods

APPENDIX J – INITIAL PROGRAMME

	2016 Q4	2017 Q1	2017 Q2	2017 Q3	2017 Q4	2018 Q1	2018 Q2	2018 Q3	2018 Q4	2019 Q1	2019 Q2	2019 Q3	2019 Q4	2020 Q1	2020 Q2	2020 Q3
LAND NORTH OF THE GRANGE - HF TIMELINE 2016-2020																
INITIAL ASSESSMENT																
NHDC APPROVAL OF SUBMISSION LOCAL PLAN																
NHDC CONSULTATION - REPRESENTATIONS																
HIGHWAY/TRANSPORT INITIAL STUDY																
ARCHAEOLOGY STUDY																
ECOLOGY STUDY																
TENDER FOR LAND CONSULTANCY																
LGCHF AGREE CORE PRINCIPLES																
NHDC SUBMISSION TO SECRETARY OF STATE																
PRE-MARKETING ADVICE																
AGREE DEAL STRUCTURE																
LEGAL ADVICE ON STRUCTURE																
SOFT MARKET TESTING BRIEF & SHORTLIST																
RIBA DESIGN COMPETITION																
PRODUCTION OF INDICATIVE MASTER PLAN																
STAKEHOLDER MANAGEMENT																
DESIGNING GOVERNANCE & IMPLEMENTATION MODEL																
COMMUNITY CONSULTATION PH 1																
SOFT MARKET TESTING																
AGREE APPROACH/FINAL MARKETING STRATEGY																
MARKETING																
MARKETING MATERIALS																
STAGE 1 - LONG LIST																
STAGE 2 - SHORT LIST																
RECOMMENDATIONS & LGCHF APPROVAL																
AGREE FINAL TERMS																
FINAL AGREEMENT																
PLANNING STAGE																
NHDC PRE-APPLICATION ENGAGEMENT																
HIGHWAY/TRANSPORT ASSESSMENT																
TECHNICAL REPORTS																
INDEPENDENT EXAMINATION																
INSPECTOR'S REPORT																
ADOPTION OF PLAN																
APPOINTMENT OF MASTER PLANNER																
CHARLETTE																
DRAFT MASTER PLAN COMPLETED																
FINAL COMMUNITY ENGAGEMENT																
COMPLETION OF MASTER PLAN																
SUBMISSION OF HYBRID PLANNING APPLICATION																

Note - NHDC Local Plan key dates in **BOLD ITALICS**

	2020		2021		2022		2023		2024		2025		2026		2027		2028		2029		2030		2031			
	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	Q1	Q2	Q3	Q4	
DETERMINATION OF HYBRID PLANNING APPLICATION																										
PLANNING CONDITIONS																										
PREPARATION FOR COMMENCEMENT OF WORKS																										
SITE COMMENCEMENT																										
ON SITE HOUSING DEVELOPMENT																										
FIRST 100 DWELLINGS COMPLETION																										

APPENDIX K – LGCHF BOARD OF TRUSTEES DEVELOPMENT PRINCIPLES**Land North of Letchworth – Key Guiding Principles**

- The development to the North of Letchworth Garden City will be built consistent with Garden City principles with the aim to deliver a high quality development of which the whole town can be truly proud.
- The Heritage Foundation will retain control throughout the design and implementation of this development and ensure that any appointed joint development partner shares our values and vision for the town.
- The new houses will be efficient and sustainable and include a mix of tenures ensuring homes are available for local people of all backgrounds and needs.
- We will actively involve the community in our planning so that they have the opportunity to engage both prior to the development of detailed plans, right through to forums as the development is being constructed.
- The development will take into account local infrastructure needs and seek to ensure that it will bring benefit to the whole town.

APPENDIX L – COMMUNICATIONS PLAN

1. OVERVIEW

1.1 Purpose

This Communications Strategy sets out how the Heritage Foundation will keep residents and stakeholders informed about and involved with the development of the land under our control included in the Local Plan, in particular the land to the north of Letchworth. It also aims to let the wider community know how they can get involved in the consultation process and learn how their feedback can shaped outcomes.

This strategy is largely focussed on the initial stages of the project and will be adapted as things progress.

1.2 Partnership working and protocols

As no decision has been made about a suitable development partner, this strategy will be reviewed to reflect how we will work with them to deliver consistent and timely communication to all those involved and/or impacted by this project. It will also be updated as the project progresses through its different phases up to and beyond homes being built.

We will work with them to agree a media protocol, which will outline our approach to dealing with the media.

1.2 Communication principles

The guiding principles for all our communications will be:

- Regular and consistent
- Honest and clear
- Plain English without jargon
- Balanced communications across suitable channels (be as social as possible)
- Flexibility
- Creativity

2. APPROACH

2.1 Vision

We will use our communications channels to set out a clear vision of the opportunity we have to create something special for the world's first garden city – a new community for Letchworth with beautifully designed homes for local people that are sustainable, eco-friendly and affordable. Our vision is also about creating successful communities for the different stages for life, with families, young adults and older people living in an environment that offers a good quality of life with green open spaces and good facilities. Most of all, we want to create a vision that shows all these new homes are very much connected to the rest of the garden city, in particular the development to the north of Letchworth.

2.2 Values

The Foundation is committed to five guiding principles or values that will underpin our approach. These principles are:

- The development to the North of Letchworth Garden City will be built consistent with Garden City principles with the aim to deliver a high quality development of which the whole town can be truly proud.
- The Heritage Foundation will retain control throughout the design and implementation of this development and ensure that any appointed joint development partner shares our values and vision for the town.
- The new houses will be efficient and sustainable and include a mix of tenures ensuring homes are available for local people of all backgrounds and needs
- We will actively involve the community in our planning so that they have the opportunity to engage both prior to the development of detailed plans, right through to forums as the development is being constructed.
- The development will take into account local infrastructure needs and seek to ensure that it will bring benefit to the whole town.

2.3 Delivery

The Foundation will have overall responsibility for delivering and managing the communication strategy for the duration of the programme and will work in partnership with the development partner to ensure we are aligned to the strategy. Where required, we will also work with other stakeholders in the community to get their input and involvement.

It is anticipated that once a partner is confirmed, there will be two tiers of reporting – an operations team for day-to-day activities/progress and a project committee that will govern progress and strategic decisions. Both groups will be accountable to the Foundation's Board of Trustees, who in turn will manage the risk register.

We are committed to ensuring consistent messaging across all our channels and as with any project of this nature, we will highlight any issues that may affect the project or the reputations of those involved.

We will work hard to convey complex messaging in an easy to understand format via the most suitable channel and be clear about what is information, involvement, consultation and how specific things will affect them.

We will carry out a number of resident involvement activities across the town for the duration of the project, with a particular focus on targeted attendance for those who will be directly affected by any improvement projects.

3. KEY OBJECTIVES (Phase One)

- Set out a clear vision highlighting the unique opportunity (and potential partnership) as well as the benefits for future generations while adhering to Garden City Principles.
- Place community engagement and collaboration at the heart of our approach ensuring that the community is involved with vision setting to create a sense of ownership.
- Use key principles to demonstrate the aspirations for all sites.

- Set up clearly defined communications channels where information is easy to access and ensure mechanisms are in place for two-way communication.
- Update and celebrate progress and activities in a timely manner.

4. CONSIDERATIONS

- Selecting a development partner who shares our vision and values who's right for Letchworth will set the tone for the project.
- Partners' expectations may impact on communication.
- Partnership approach means we need to be aware of keeping messages consistent.
- Communicating decisions on infrastructure (as this has been flagged as a particular concern).
- There are reputational risks in not meeting milestones and commitments.
- The consultation process with Council and the Foundation could confuse people plus consultation fatigue could mean that residents fail to engage with face-to-face activities.
- Recognise that residents in different phases require different messages to enable them to feel they have information that is relevant to them, while at the same time maintaining overarching communications.
- Ensure that we look at national opportunities for communication.
- Communication and involvement with our staff and Governors is vital in ensuring that the right information is shared more widely and that misinformation is corrected.
- An approach to crisis communication will need to be agreed with our partner.

5. OVERARCHING KEY MESSAGES (Phase one)

- This is an opportunity for the Foundation to partner with an organisation that shares our vision, values and ambitions for Letchworth – we will not sell off the land to the highest bidder – we will have full control over what is built.
- This will be a unique opportunity to design and build quality homes that are comfortable and efficient using bespoke design principles to ensure quality building and planning.
- There will be different tenures to meet the need of local people while having 40% affordable homes (quality, suitability, affordability and sustainability).
- Conversations with the community and stakeholders are vital to help us understand every issue to create a better place to live with training and employment opportunities for those who will live here and a place that works for Letchworth as a whole.

6. AUDIENCES

Our audiences will have different levels of interest in this project so it's important that we carry out a stakeholder mapping exercise in early 2017 to identify which groups have low, medium or high interest. Our messaging will need to be adapted for the different audiences which include:

- Letchworth residents, in particular those living in particular the Grange estate
- The wider Letchworth community including key local groups (e.g. Transition Town Letchworth, Letchworth Arts & Leisure Group, Garden City Society)
- Service providers (e.g. NHS, Public Health, GPs and charitable sector)
- Herts County Council and North Herts District Council (key departments such as highways, education, local services, leisure, waste and the environment)
- Local ward councillors and cabinet members (to understand timescales and impact on their constituents)
- Wider political stakeholders
- Local faith organisations (churches and temples offer another opportunity for valuable face-to-face communication)
- Town & Country Planning Association
- Local schools and North Herts College
- Local businesses with an interest in the town's future workforce (including the Business Improvement District)
- National Heritage Groups (eg William Morris Society, 20th Century Society)
- Local, regional and national print and broadcast media (build relationships to make it easier to proactively place stories plus responses to news stories generated externally, opinion pieces and general enquiries about progress).

6.1 Residents and stakeholders

- Provide accessible and meaningful information that tackles concerns about building on industrial sites and greenbelt land, population growth and infrastructure.
- Provide appropriate and timely information on consultation activities and opportunities to influence building and service design.
- Provide timeline information in a meaningful manner, which makes it clear where timescales.
- Provide appropriate and timely messages to residents affected by development work, with clear feedback loops to report disruption or other issues.

- Promote the benefits for the local community, including employment, community development activities and new facilities.

6.2 Community groups

- Demonstrate how the development will benefit the whole of the Letchworth community.
- Involve groups in engagement activities and respond to their concerns.
- Consider appropriate channels to keep them informed and involved.

6.3 Political / local government

- Provide appropriate and timely information on issues affecting local residents and mitigation of any issues raised with ward councillors.
- Promote the partnership approach and highlight affordable homes for local people and any community investment/employment programmes.
- Consider which events and activities might be of interest to councils and MPs.

6.4 Local businesses.

- Promote opportunities for businesses to be part of this new community.
- Get them involved with shaping Letchworth's future workforce.

6.5 Print and broadcast media

- Cultivate good relationships with local reporters and national journalists to build trust, encourage honest media coverage and the ability to comment (go to them in person where possible).
- Promote progress, events and successes through press releases and updates to website and social media.
- Provide quality, usable content by using video, audio and infographics.
- Be reliable and responsive by providing statements within 24 hours in response to media enquiries.

7. CHANNELS

It's important that we use a variety of communications channels and tailor our communication to the right audience. These will include:

- Printed materials such as the Foundation's Newsletter as well as flyers and posters.
- Face-to-face, including resident-to-resident communication where possible.

- Online communications via a dedicated website plus the Foundation's website and partner websites.
- Social media using video, audio, animation and user generated content.
- Email and phone – a dedicated email address and phone number will be agreed.

7.1 Face-to-face contact

Face-to-face contact is vital to build relationships, get a feel for the concerns within the community, for the community to get a true sense of what is really happening, get to know the people behind the project and to correct any misinformation. We need to pull together a programme of face to face opportunities where things are already planned or where we need to be more proactive and arrange face to face opportunities (see 7.4 and 7.10).

7.2 Letters

Letters are still a vital channel, particularly when important news is being communicated to individuals that requires a personal approach.

7.3 Newsletters

The Foundation's newsletter is published twice a year so this may not be enough once the project progresses and more community engagement is needed. We should consider whether or not we produce a specific newsletter for this project.

7.4 Local meetings and surgeries

As mentioned in 7.1 face-to-face contact is the most valuable way of building relationships and meetings and dedicated surgeries are a great way of reaching those we need to engage with. We will seek opportunities to attend local meetings and set up special surgeries where people can come and speak to the team involved.

We will encourage residents to use our open sessions to discuss the project and we will speak to faith groups about attending suitable events to reach out to the wider community.

We will continue to work with our own Governors by providing updates to them at Governor meetings and giving them the tools they need to have conversations in their communities.

7.5 Pop-up events

Pop-up events are a great way of attracting younger people to get involved in the community. These are street events where we can speak to passers-by who may not otherwise engage with consultation. The events will raise awareness of the project and using interactive techniques on i-Pads and boards, we can gather people's views and their aspirations for the future.

We can also involve our venues and services in hosting pop-up events or dedicated smaller scale community festivals or projects e.g. The Collection could run an oral history project with local people talking about their memories of growing up in Letchworth and their hopes for the future. Young people in particular could collect video footage about 'new beginnings' in Letchworth and this could be made into a montage for using on social media.

Broadway Gallery and Theatre could run art and drama workshops relevant to the project. This could culminate in an exhibition or performance. Broadway Cinema could do a small outdoor screening on the Grange estate and the team from the Ernest Gardiner Treatment Centre could run free health checks and balance assessments for older people.

7.6 Website

A well-designed website will be launched for the main development at the Grange and will be maintained by the Foundation's communication team.

It will contain regular updates and information about how to get involved in consultations, news updates, reports and records of all public documents. To appeal to all ages, it will feature videos, animations and infographics. Some information will be on our corporate website but will focus on signposting to the site.

7.7 Social media

Although social media management takes up a huge amount of time and resource, the community, particularly younger people, will expect to have conversations with us online and for their queries to be responded to quickly.

We need to consider how and when we would introduce a social media account dedicated to the project. This social media account would be used to broadcast and record news, updates, Q&As and events.

7.8 Community noticeboards

We will use existing noticeboards to share key information and consider if more boards are needed in other locations.

7.9 Events

Events will bring people from across the community together and can be used to raise awareness of the project.

This includes the Letchworth Festival, Food & Drink Festival, Standalone 10K and other events organised by the BID. We will also consider speaking to parents who attend events at Standalone Farm.

7.10 Design Competition

We propose holding a design competition that will engage the community in 're-imagining the Garden City' for the 21st Century.

The site to the north of Letchworth will be the case study and we will ask architects and designers to develop concepts with a modern interpretation of early Garden City Design Principles, to ensure that the design of new development is reflective of today's needs, while creating great places to live. Entries to the design competition will be used for an exhibition and community engagement/discussion and act as a catalyst for inspiring a collaborative approach.

7.11 Phone and email contact

We will host a dedicated email address for people to get in touch about any issues relating to the consultation and involvement in the project. A phone number will also be made available for those who wish to speak to someone confidentially or in more detail.

8. COORDINATION ISSUES

8.1 Central communications

The Foundation's communications team will have responsibility for the delivery of this strategy and the plan (when specific activities are agreed.) Further discussion is needed on how the day to day communication will run (see 9.0) but the team will support coordination with our partner as and when needed.

8.2 Residents and stakeholders

Issues will be discussed at pop-up surgeries and open sessions but consideration needs to be given to utilising an existing residents' liaison group, or setting up a new one, specifically for this project where issues can be resolved and disseminated to the community. Resident to resident communication is powerful and can often address matters before they become problematic.

8.3 Construction progress

We are committed to working with a partner who will sign up to our principles and work with the community in a proactive and respectful way. The partner will work with the Foundation to share important updates with local residents. They will also be required to take part in community engagement activities.

8.4 Complaints

We will have a clear complaints process on our website and in printed materials.

9. MEASUREMENT AND EVALUATION

It's important that we regularly monitor the impact of our activities and this will be done in team and project meetings. There are a number of tools that we can use to help us.

These include:

- Daily media and social media monitoring.
- Website traffic will be measured via monthly, quarterly and annual google analytic reports.
- Enquiries to dedicated inbox.
- Attendance at events and engagement activities will be monitored along with anecdotal feedback.
- Surveys to measure local perception and understanding of the project.
- Feedback from Governors and other stakeholders.